

Rosslyn Sector Plan Update

Existing Conditions Summary

June 7, 2013 | DRAFT

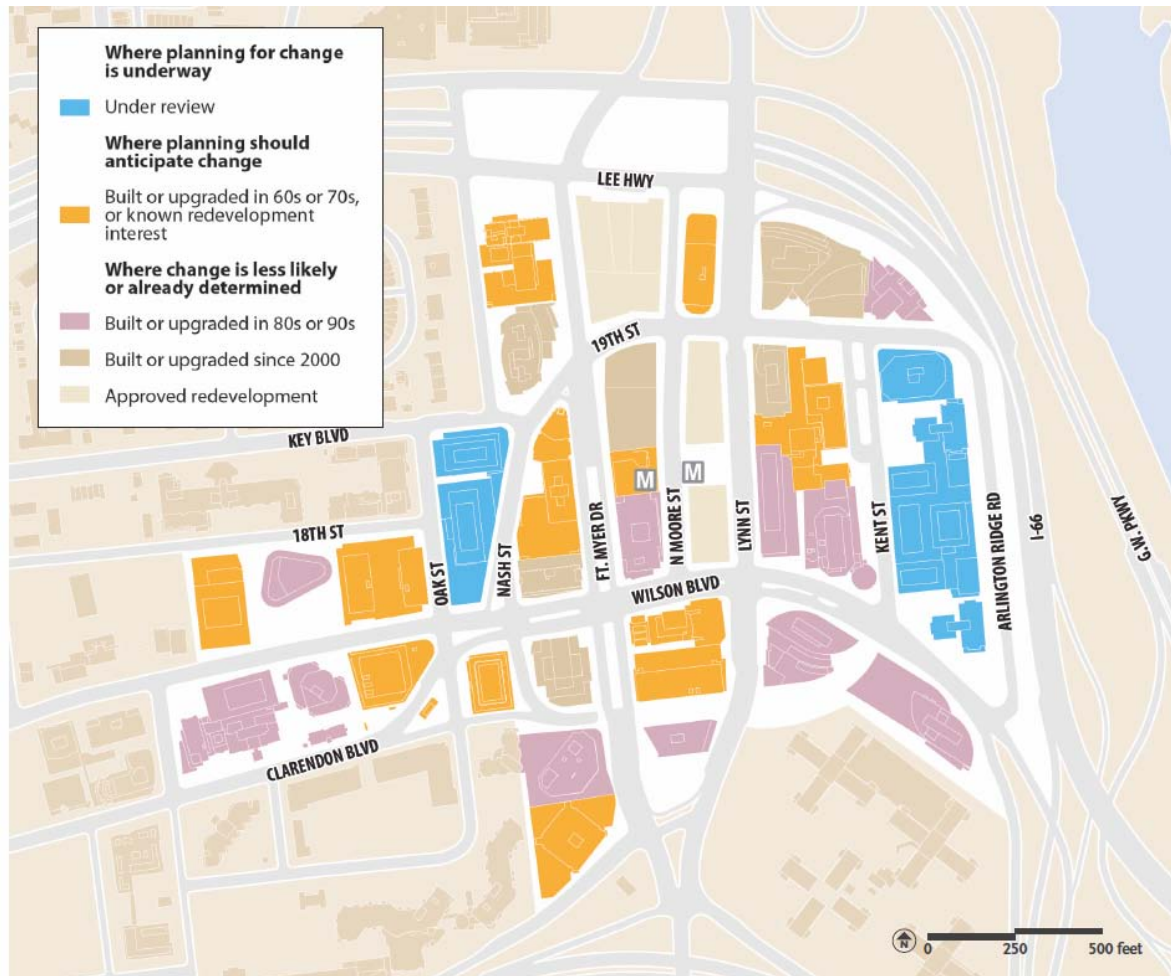
A. Introduction

This document is intended to summarize the major findings from the existing conditions analysis completed to date in support of efforts to develop a Rosslyn Sector Plan Update. As part of the staff and consultant team's efforts related to Realize Rosslyn, existing conditions have been looked at within the four areas of work that frame the scope for this effort: transportation; public parks and open space; urban design; and building form and height. The analysis of these existing conditions is an important first step in beginning to develop some preliminary ideas and potential conceptual proposals as part of Realize Rosslyn. While a series of other inputs, such as community input, stakeholder interviews, comparative analyses, will help to determine the direction and ultimate recommendations and ideas put forth in the Sector Plan Update, the existing conditions analyses establish a baseline condition that can influence how ideas emerging through the planning process have come about, and what they are based on.

B. Likelihood of Change

This section begins with analysis and identification of sites that are more and less likely to receive significant investment in redevelopment over the course of this plan's 20-year horizon. While many factors will affect the timing and sequence of redevelopment on major sites in and around the study area, reasonable predictions can be made as to likelihood of change based on known plans for certain sites and the age and condition of buildings on other sites. The diagram below identifies the general likelihood of change on privately-owned sites in the RCRD and how this planning process should thus consider opportunity for change. Legend category distinctions are described below.

Likelihood of Change Map



Legend categories addressing how planning should anticipate change on different sites:

- Where planning for change is underway
 - Under review: Rosslyn Plaza is currently in the design and review phase as developers work internally and with the County to refine a major phased redevelopment proposal for the large site. A redevelopment proposal has also been submitted recently for the block containing 1400 Wilson Boulevard and 1401 Key Boulevard.
- Where planning should anticipate change
 - Built or upgraded in the '60s and '70s/known interest in change: Many of Rosslyn's key parcels are currently occupied by aging office and other building types that are increasingly being outmoded by new developments around Rosslyn and the region. Several of these sites will probably redevelop in the near future, presenting major opportunities to transform large sections of Rosslyn's core with more up to date buildings and urban settings. In some cases, property owners are in the early stages of contemplating redevelopment. A major goal of the Realize Rosslyn planning process is to

coordinate these emerging redevelopments and maximize their contribution to a livable public realm.

- Where change is less likely or already determined
 - Built or upgraded in the '80s and '90s: Located particularly along parts of Wilson Boulevard are buildings that were either built or significantly renovated recently enough that they will likely maintain market competitiveness and economic viability for years to come. While long-term goals might be set for their distant redevelopment, it will be more important to address near-term opportunities to better integrate these buildings into a more livable urban fabric.
 - Built or upgraded since 2000: Buildings completed or substantially renovated in the past 15 years should be considered permanent for purposes of this plan, and all efforts should be directed towards best integrating them into Rosslyn's emerging urban form.
 - Approved redevelopment: These projects are designed and have been approved by the county for construction. While their sites are now changing or will soon, their form is essentially determined already for the purposes of this planning process. 1812 North Moore Street is currently nearing completion. One of the two Central Place buildings may begin construction in 2013-2014. The second Central Place building and/or portions of Rosslyn Gateway may begin construction within approximately five years or as market conditions allow.

C. Urban Design Existing Conditions

The most dominant issue in Rosslyn's urban design conditions today is its incomplete transition from a place designed around cars to one designed for people. The people are present in great numbers, walking to and from Rosslyn's metro station and its many other significant locations for working, living, leisure and travel. The streets and paths they walk on, however, largely remain indifferent to pedestrians' needs for safe, pleasant places to walk: blank walls and parking entrances predominate, while welcoming shops and cafes are rare. Street trees and vegetation offer shade, scale and beauty in certain areas, but many streets still have little or no landscape oriented to the pedestrian. While walking is of course an important aspect of transportation, and addressed as such in that section below, it also fundamentally affects and is affected by quality of place, and thus a key focus of urban design considerations.

An additional legacy of Rosslyn's development patterns in the 1960's and 1970's is its segregation of places for working, living and shopping in different areas. While more recent development in central Rosslyn has begun to mix these activities, there remain blocks heavily dominated by workplaces only. While workplaces are very important and desirable in Rosslyn, areas where they dominate become as empty on evenings and weekends as they are busy on a weekday morning, preventing Rosslyn from achieving its full value as a great place to be. Thus far, the lack of a critical mass of housing located within Rosslyn's core may also pose challenges to establishing a sense of neighborhood community among the few residential buildings located in the RCRD. Finally, internal retail spaces reduce the amount of publicly visible retail choices and pedestrian activity on Rosslyn's streets. The next series of diagrams will map these conditions across the study area.

Pedestrian traffic

The diagram below indicates the relative level of pedestrian activity on different street segments in Rosslyn (data courtesy of Monday Properties and Streetsense). While many segments receive heavy foot traffic because they connect the Metro station with office lobbies, their sidewalk conditions do little to make walking a pleasant experience.

Existing Pedestrian traffic



Ground Floor Building Edges

Ground floor building edges throughout Rosslyn’s RCRD show a wide range of physical and use characteristics, from active retail and non-retail uses, to places for public art, blank walls, garage and service access points and activities. Notably, most streets have a disorganized mix of these elements, causing loading docks or blank walls to intrude between lobby entrances and restaurants. When most every street plays such a role of both front door and back door, no street offers clear appeal for walking. The image below depicts ground floor building edge conditions that exist in the RCRD today or are already planned. The active retail element of these edges is isolated and discussed further in a subsequent diagram.

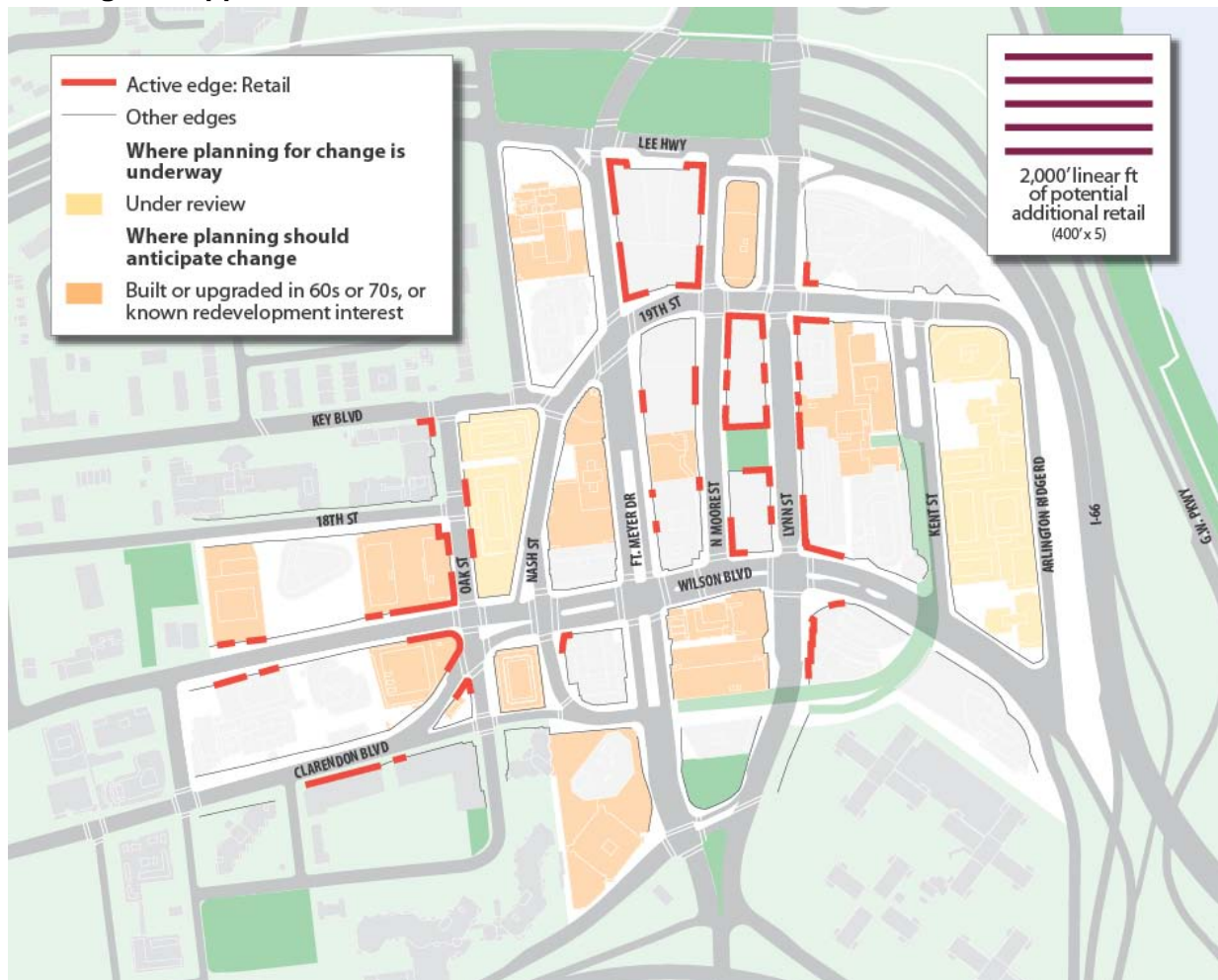
Existing and Approved Ground Floor Building Edges



Active Ground Floor Retail

A key element of providing an enhanced walking environment in the RCRD is to identify where ground floor retail should be concentrated to create strong retail districts in Rosslyn. Plans for where retail storefronts should be in the future must take into account where retail exists today and where it is expected in approved redevelopment, as expressed in the Active Ground Floor Retail map exhibit that follows. Planning for retail also must recognize that retail market demand, even in a busy district like Rosslyn, has its limitations, particularly when people are accustomed to making purchases in other more established retail centers and online. For the purposes of this plan, to be conservative, it is assumed that up to about 2,000 additional linear feet of retail storefront can be anticipated in Rosslyn though about 2030. This storefront length translates to about 100,000-120,000 sf of retail floor area assuming average space depths of 50-60 feet. This floor area is roughly equivalent to the approximately 200,000sf of retail growth forecast in the RCRD by Arlington County through 2040, less the retail floor area expected in approved development projects. While retail beyond this amount should be welcomed, it should also not be assumed, to avoid policies that try to force retail to occur in amounts or locations where it is not feasible.

Existing and Approved Active Ground Floor Retail



Planning should also recognize that retail business success is affected significantly by location, with ease of visibility and access, and clustering with other retail, usually important prerequisites. Other considerations for prioritizing future retail locations include streetscape conditions and availability of retail-appropriate building space. Rosslyn's topography significantly affects streetscape and space availability, as steeper grades tend to discourage pedestrian traffic, and tend to result in building floor plates that either step up the slope in small increments – often too small or inflexible for retail – or step in large increments – leaving long stretches of floor structure and spandrel wall between infrequent storefront windows and doors. From a phasing perspective, attention is needed into how active ground floor uses in existing and new buildings can be integrated into a coherent network over time.

Non-retail ground floor uses

Because it is unlikely that retail storefronts will line more than about a third of street frontage in the core study area, programming and design for other ground floor uses will continue to have an important impact on street character. Principal other ground floor uses with regular occupancy by people (e.g. other than servicing and parking) include

- Entrance lobbies to office and residential buildings
- Office space
- Housing (on peripheral streets around Rosslyn's core)

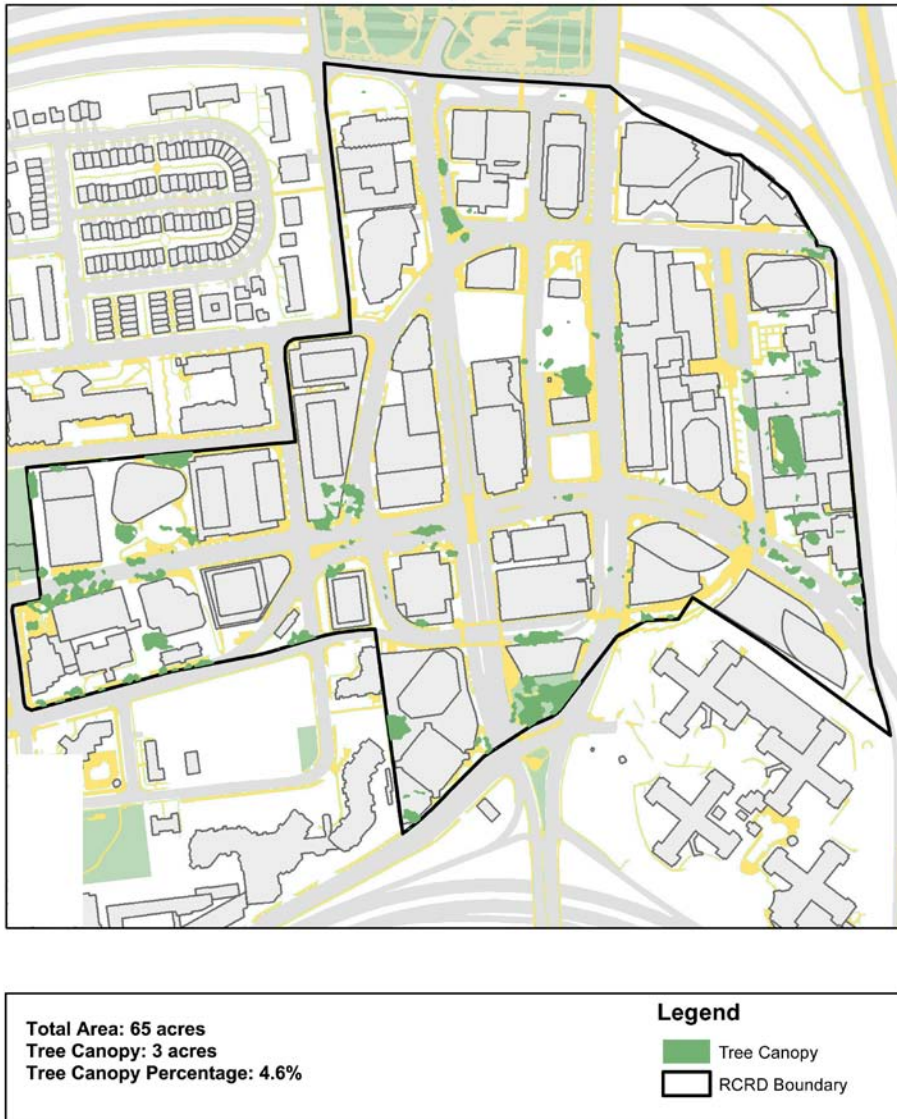
Entrance lobbies deserve highly visible, accessible and well-designed placement on streets to best leverage the high property value and diverse access choices that are some of Rosslyn's strongest assets. Office space and (away from Rosslyn's core) housing are desirable as ground floor uses where more active ground floor uses are not likely. Looking forward, it would be desirable to expand the range of non-retail uses that can provide pedestrian-friendly sidewalk edges. Existing or potential examples include child care, arts uses such as galleries, and live/work or studio space. These spaces can also be designed with key spatial characteristics allowing potential future conversion to retail, such as high ceilings and level sidewalk entrances.

Urban landscape

Trees and other plantings play important roles in and along urban streets. The presence of street trees reliably increases value of adjacent properties. Street trees also shade streets, making them comfortable for walking and reducing the "urban heat island effect" by preventing pavement and other hard surfaces from absorbing solar energy. Street trees also provide an important scale transition from the human body to the larger forms of tall buildings.

Tree Canopy Coverage, RCRD

source: Arlington County, Rosslyn Briefing Book, p. 12

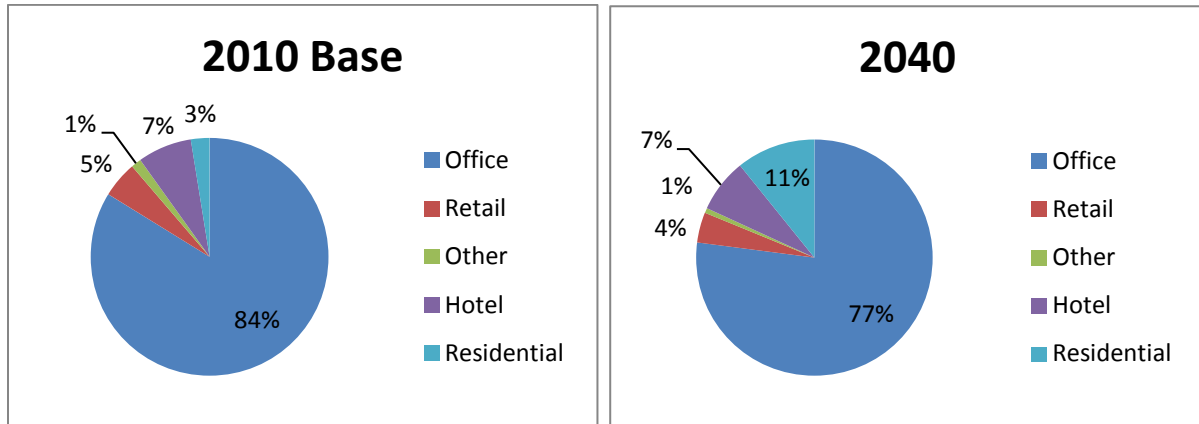


Beyond plantings, the programming and design of sidewalk spaces can strongly influence their appeal as places to walk and gather. Rosslyn is distinguished by its strong tradition of public art installations along its streets and parks. There is opportunity, though to extend public art to more places in Rosslyn as well as to make it more changeable, to spark interest through its change and also its response to the ideas of the moment. The planned Corridor of Light installation along North Lynn Street will address these opportunities through an emphatic day and night presence along one of Rosslyn’s most regionally-prominent streets and through the extensive variation possible in its light displays.

Urban landscape can also tell important local stories through signage. Rosslyn has a couple of such examples: one memorializing the invention in Rosslyn of the “Arpanet” predecessor to the intranet, and one memorializing the conversations between “Deep Throat” and Bob Woodward in a Rosslyn parking

garage that helped reveal the Watergate scandal. Again, however, there is untapped opportunity to make these and more stories much more visible and accessible to Rosslyn workers, residents and visitors. There is further need to provide better basic information on the streets for pedestrians, such as directions for the many visitors who arrive in Rosslyn looking for the Marine Corps Memorial (Iwo Jima) and the Netherlands Carillon.

RCRD land use mix, 2010 and under current Round 8.2 Arlington County growth forecast for 2040



Under the current growth forecast, housing’s share of overall development area in the RCRD will increase from approximately 3% today to approximately 11% in 2040. While this growth in housing would be substantial, it would still be heavily outpaced by office growth. The forecast anticipates that three square feet of new office space will be created for every one new square foot of housing. The plan should consider the feasibility and opportunity of increasing housing’s share of new construction in the RCRD to promote more substantial increases in retail demand, public realm activity and sense of neighborhood. For instance, if overall new building floor area forecasts remain the same but just two square feet of office space are built for every one new square foot of housing, housing’s share of RCRD floor area would increase to 14%. If 1.5 square feet of office space are built for every one square foot of new housing, housing’s share would increase to 17%. The most important question regarding feasibility of these scenarios is whether housing would command enough real estate value relative to office space to merit developer interest in building more. Ultimately, this question will be determined by market conditions over a time period that is impossible to predict with accuracy today. A major current mixed-use redevelopment proposal in Rosslyn proposes just 1.2 square feet of office space for every one square foot of housing it would create, indicating that at least under current conditions, a higher share of housing appears economically feasible.

D. Building Height and Form Existing Conditions

Building construction continues daily in Rosslyn, with the district's newest, tallest building – 1812 North Moore – having been topped out during the planning process so far, and construction expected to begin within the next year on portions of Central Place. Major redevelopment proposals for Rosslyn Plaza and the 1401 Wilson Boulevard/1400 Key Boulevard block are under discussion, and others for Rosslyn Gateway have been approved. This activity is evidence that Rosslyn's earliest generation of office and residential buildings no longer delivers the real estate value potential it once did. With a number of older buildings no longer competitive in the marketplace due to low ceilings, small floorplates and dated architecture, and County willingness to allow more development floor area on sites (up to FAR 10) than many currently contain, property owners have incentive to rebuild.

This redevelopment activity offers the important opportunity to improve the unwelcoming pedestrian conditions described in the evaluation of Urban Design issues above. Moreover, it offers opportunity for enhanced architectural quality and distinction that could turn Rosslyn's prominent but undistinguished skyline into a source of value and pride. In recent redevelopment discussions, County policy has aimed to achieve these goals, ensuring that the new real estate value created also come with important improvements – like ground-floor retail and more attractive building form – having value to property owners as well as the general public. Current policy, however, would benefit from more specific development and design criteria for building height and form than are currently in place.

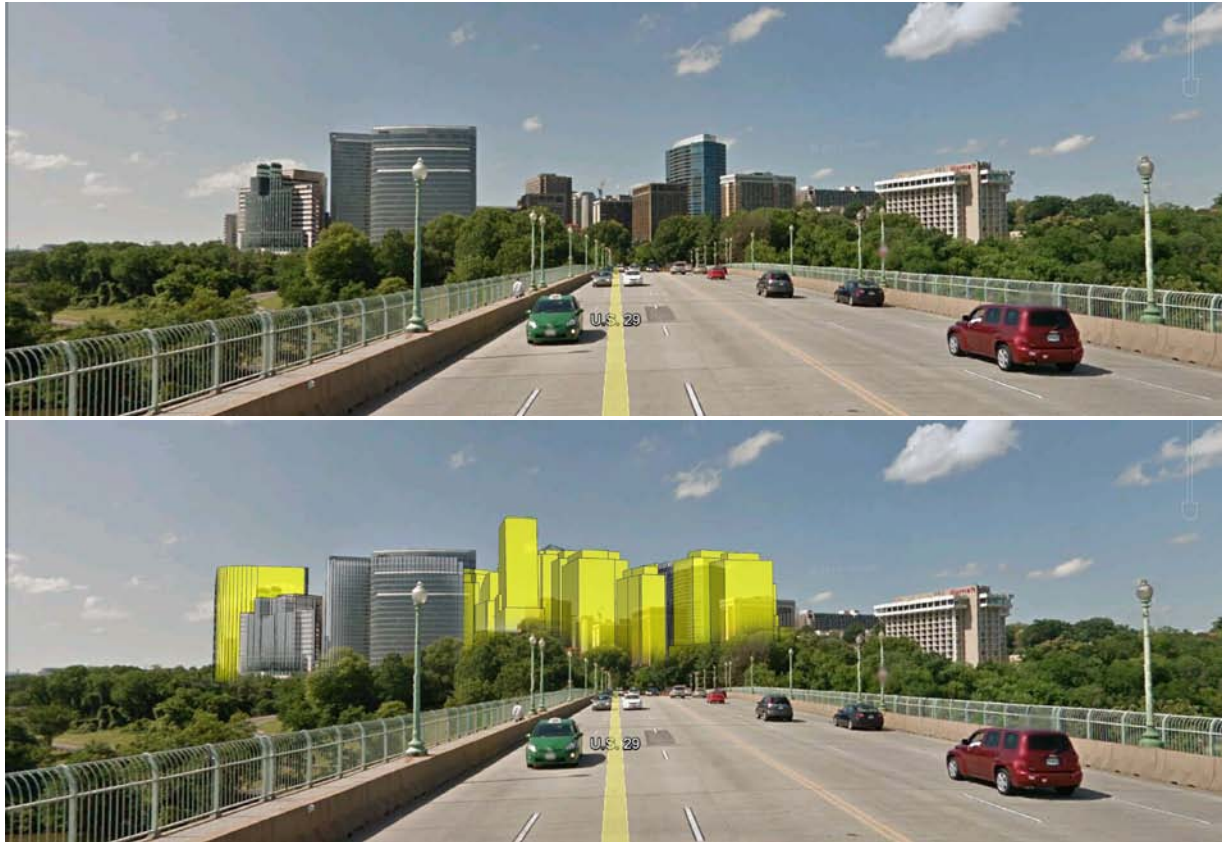
Important building height and form issues requiring attention in this plan include:

- Defining how buildings should transition in scale as they reach the street and adjoin traditional lower-scale neighborhoods. Significant building heights and widths can make pedestrians feel uncomfortable and produce abrupt changes between the character of adjacent blocks and buildings.
- Determining how building height should be modulated within Rosslyn. Established County policy welcomes the area's greatest building heights on sites immediately around the Metro station and calls for heights to taper down in all directions from this high point. The profile of this taper, however, is not specifically established, except that buildings in the core have been allowed to rise to the range of 370-390' above average site elevation while other sites have been limited to 300'. The greatest heights, and a lower height tier east of North Lynn Street, remain constrained by the Federal Aviation Administration (FAA) and Metropolitan Washington Airports Authority (MWAA) due to presence of flight paths approaching Reagan National Airport. The distinction between the 370-390' and 300' height ranges is modest, diminishing perception of the desired taper.
- In a further challenge for the tapering policy, theoretical development capacity is equal on sites across the district, creating incentive for sites with lower height allowances to have broader, deeper buildings. In practice, development potential on major development sites has varied from the consistent FAR 10, with some sites acquiring development potential from other sites. Thus the plan must anticipate the possibility, and potential desirability, of future density transfers of this sort as part of determining appropriate building height relationships. Any such density transfers

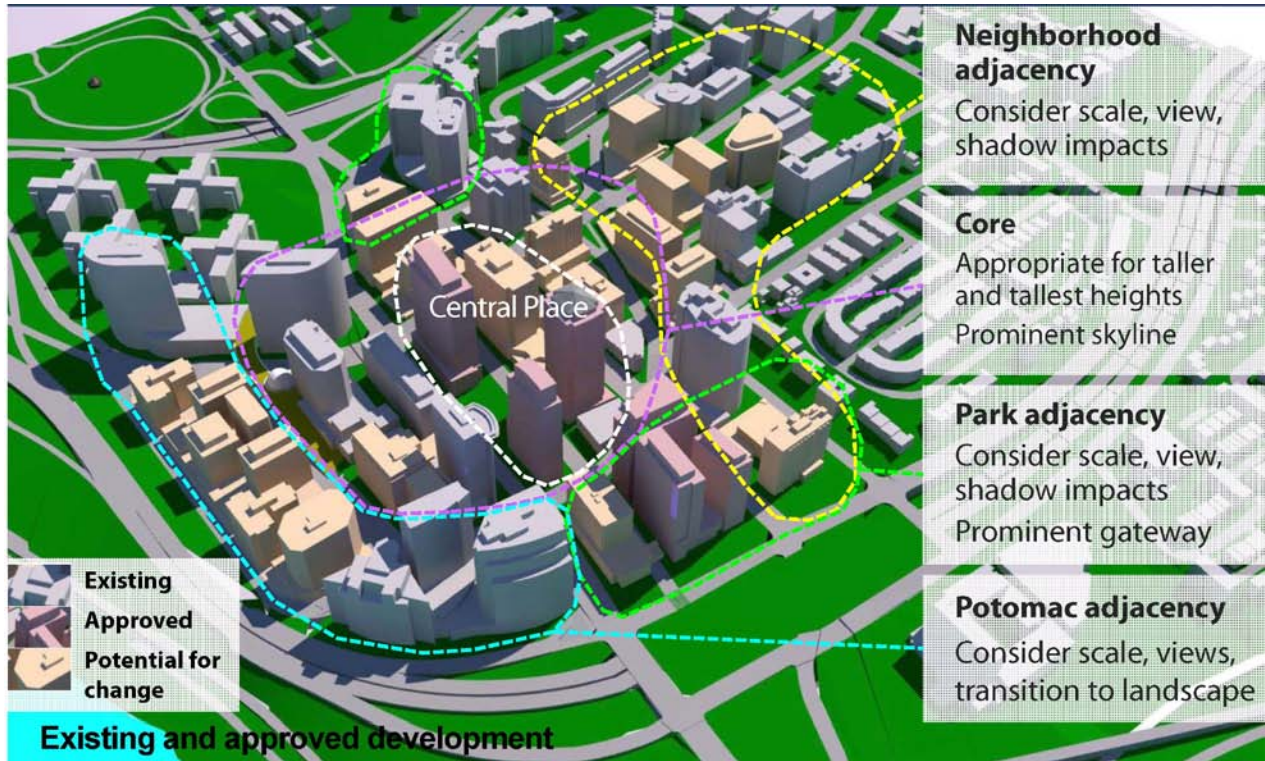
and development capacity changes should, however, be accommodated in a manner that is equitable to affected property owners.

- Building height and form impact environmental comfort as well as appearance. Significant vertical and horizontal building dimensions produce shadow and wind impacts on adjacent parcels and public spaces that may be undesirable, and may need to be mitigated.
- Dramatic views are available from many public and private locations in Rosslyn, contributing significantly to quality of place and development value, but remain highly vulnerable to being blocked by new building masses. Building form and height policy should thus consciously preserve and expand the range of views possible. As an example, the public observation deck planned for the top of the Central Place office tower will have commanding views of Washington, DC's Monumental Core, the Potomac River, Arlington National Cemetery and other landmarks, but only as long as future development leaves those view corridors open.
- In some cases, increased building height may enable a reduction in building footprint or width that creates space at ground level for public parks or street connections. The approved Central Place project follows this approach. The plan should identify whether there are other locations or conditions meriting such an approach.
- Rosslyn's compact area, firm edges facing the Potomac, consistency of height and massing among many existing buildings, and street geometry cause its buildings to read strongly as a solid, relatively impenetrable mass from afar. Future development opportunities will tend to reinforce this appearance as they fill in remaining gaps in the skyline and between existing buildings. To ensure Rosslyn maintains a diverse and welcoming character, planning should aim to prevent this monolithic appearance with strategies such as introducing space between buildings at Rosslyn's edges, and introducing material variety and depth into building facades.

Simulation of potential rough building massing from Key Bridge: existing and potential views (note: lower image is not intended to represent a preferred development scenario, and is for illustrative purposes only)



The simulated view above, which presents one scenario for new development that maximizes available development height on sites in Rosslyn’s core, demonstrates that new buildings may tend to overlap one another and reach similar heights. This raises the importance of introducing depth and variety into building façade planes – individually and collectively – to prevent Rosslyn from appearing a monolith.



E. Transportation Systems Existing Conditions

Ease of access is at once one of Rosslyn's greatest assets and challenges. In addition to its proximity to downtown Washington, D.C., Rosslyn's establishment as a major office and hotel district in the 1960's and 1970's arose directly from its convenient auto access via a variety of regional highways that were built or enhanced in that era. This automobile access remains important to real estate value and market potential in Rosslyn. Although the planned loop road within Rosslyn's core was not fully implemented, other streets in Rosslyn continue to provide effective driving access to its properties. Rosslyn's roughly 22,000 parking spaces (roughly 1.25 spaces per 1,000sf of gross building floor area, on average) are reportedly well used at peak weekday hours. New development projects, though built to lower parking ratios than past projects, still tend to include significant amounts of parking. Recently built and approved development projects in central Rosslyn have often located much of this parking above grade, above the ground floor and surmounted by occupied floors, because sub-surface rock dramatically increases the costs of building underground parking more than about two levels deep.

The opening of the Rosslyn Metro station in 1977, however, transformed Rosslyn into a place of multimodal access choices, and dramatically expanded access capacity with very little use of land. Metrorail boardings have grown significantly over time, causing Rosslyn to be the busiest station outside of the District of Columbia. The station also functions as a major bus hub and transfer point for WMATA and ART, as well as a destination for numerous private buses and shuttles serving local institutions and hotels as well as long-distance commuters. Increase in Metrorail and bus usage has contributed to the significant pedestrian activity described in the Urban Design section above.

Rosslyn also sees significant biking activity, largely because major regional bike routes (the Custis and Mt. Vernon trails, as well as the Key and Theodore Roosevelt Bridge crossings) pass by its edges. Installation of four Capital Bikeshare stations in Rosslyn has further spurred increases in biking. Biking conditions on Rosslyn streets, however, remain challenging.

This evolution of Rosslyn into a multimodal hub has resulted in the following set of principal transportation issues that frame this planning effort:

- The majority of peak-hour traffic on Rosslyn streets -- about two-thirds -- is passing through Rosslyn on its way somewhere else (primarily to or from the District of Columbia). A significant portion of this uses Rosslyn streets to connect from one regional highway or bridge to another, due to the absence of certain direct connections among those routes.
- Heavy pedestrian traffic in Rosslyn deserves safer street crossings, a more inviting sidewalk environment, and more direct routes that penetrate "superblocks" with block lengths exceeding 600 feet.
- The bike network needs much safer and better-marked internal connections on Rosslyn streets, linking to adjacent regional trails serving recreation and practical transportation needs.

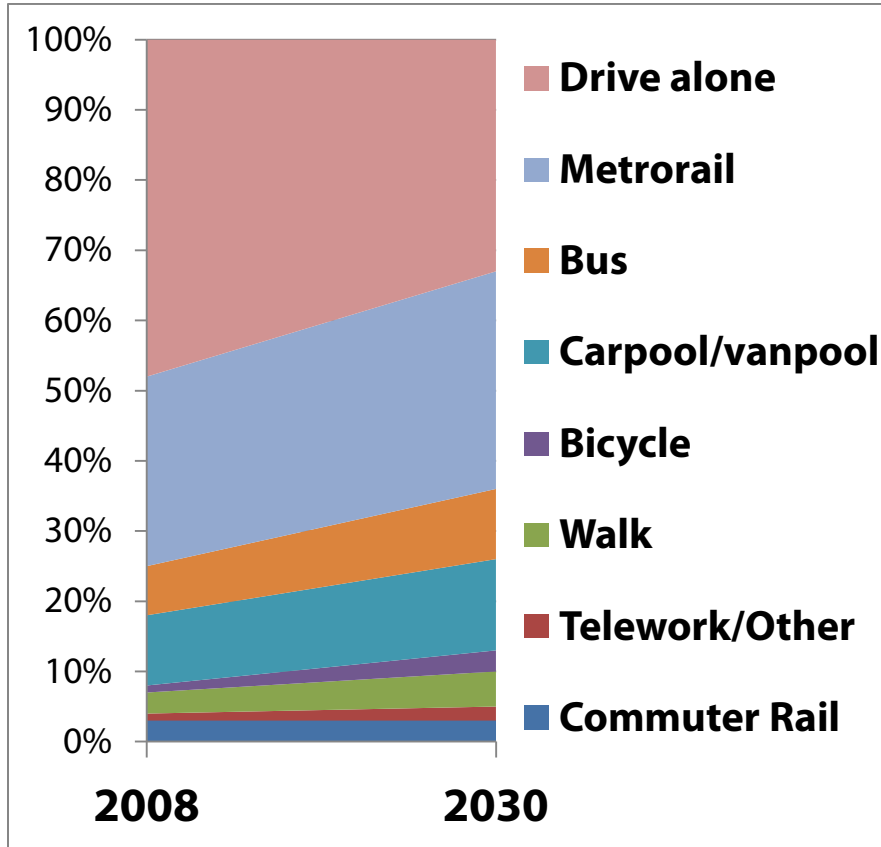
- Metrorail capacity expansion is limited, as the tunnel under the Potomac already carries as many trains as it can at peak periods. Extending orange line train lengths from six to eight cars offers the main near-term opportunity to expand capacity. Anticipated opening of the Silver Line in late 2013 will further constrain capacity. Long-term capacity enhancement concepts include constructing a second rail tunnel and station under North Fort Myer Drive accommodating Blue Line service (initially connecting south only, with possible extension into the District of Columbia via a second Potomac tunnel). This concept would include below-grade connections to the existing station platforms, and would likely need another station entrance(s) and ventilation shaft located along North Fort Myer Drive. Another concept, less favored by WMATA, would connect the southern Blue and western Orange Line corridors on the south side of Rosslyn, and would likely bypass Rosslyn without stopping.
- Bus services offer the most flexible near- to mid-term opportunity to expand transit service capacity and choices. WMATA anticipates a bus ridership increase of 1% per year in Rosslyn, and an ultimate need to increase service frequency up to 50% on major lines. Increasing bus capacity will require adding and consolidating bus stop locations. Creation of a dedicated off-street bus station, such as beneath North Fort Myer Drive (as part of a rail tunnel project) or as part of a redevelopment of nearby properties, could help, given other pressures on curb space in Rosslyn. New stop locations could be provided with the proposed two-way conversion of North Fort Myer Drive and North Lynn Street.
- The District of Columbia’s planned streetcar system could conceivably be extended to Rosslyn across the Key Bridge, a route used by streetcars in the past. Adding streetcar service in Rosslyn as a long term venture could significantly promote retail activity, particularly along North Fort Myer Drive and add valuable connections to Georgetown. To preserve opportunity for future streetcar service, Arlington County should actively participate in the District of Columbia’s streetcar planning efforts, and preserve potential for a streetcar connection between Georgetown and Rosslyn via the Key Bridge.
- “Slug lines,” an informal car pool system with pick-ups and drop-offs at several locations in Rosslyn, help reduce traffic in Rosslyn and on regional highways. The approved Rosslyn Gateway redevelopment project would displace the current slug line curbside stops at the edge of that site. This plan should determine an appropriate new location(s) for that and other slug lines (as needed) that is balanced with other demands for curbside spaces such as public parking, shuttle stops and taxi queues.

The 2012 Rosslyn Multimodal Transportation Study (RMTS) addresses these and other factors with a variety of proposed solutions. The most significant proposals, in terms of benefits as well as pure change, include:

- Convert North Fort Myer Drive and North Lynn Street to two-way traffic
- Extend 18th Street from North Lynn Street to North Arlington Ridge Road
- Extend 18th Street from North Oak Street to North Fort Myer Drive
- Remove the North Fort Myer Tunnel at Wilson Boulevard
- Relocate bus bays from North Moore Street to North Fort Myer Drive

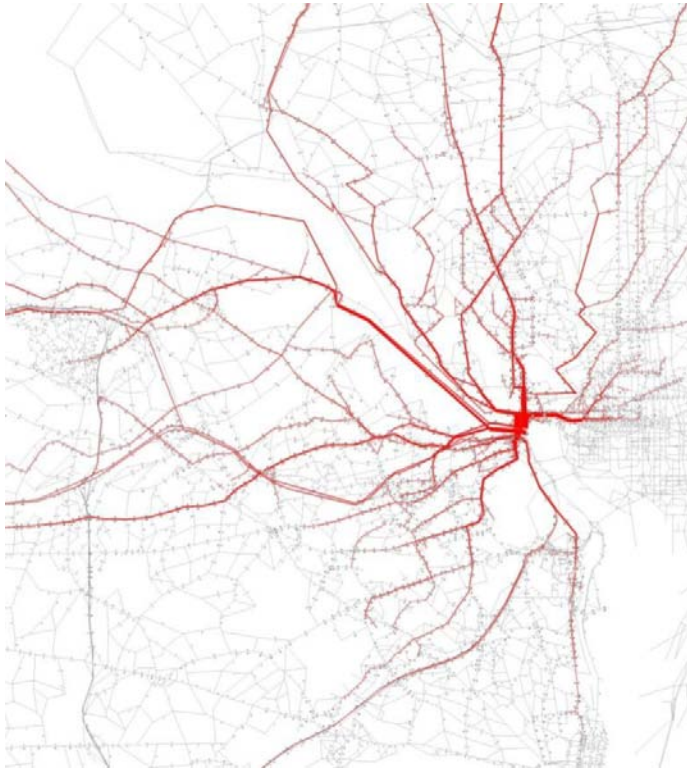
Analysis conducted as part of this planning study reviews the RMTS recommendations, conducts more detailed analysis on selected issues, and suggests an updated set of transportation recommendations.

Current and proposed commuter transportation mode share distribution



2008 figures reflect actual usage. 2030 figures reflect Arlington County DES recommendations on the mode share needed to accommodate County development growth forecasts while limiting traffic increases to 5% over 2005 levels. Significant and deliberate policies would likely be needed to achieve this shift away from single-occupant vehicles toward transit, biking, walking and other alternatives requiring less roadway space.

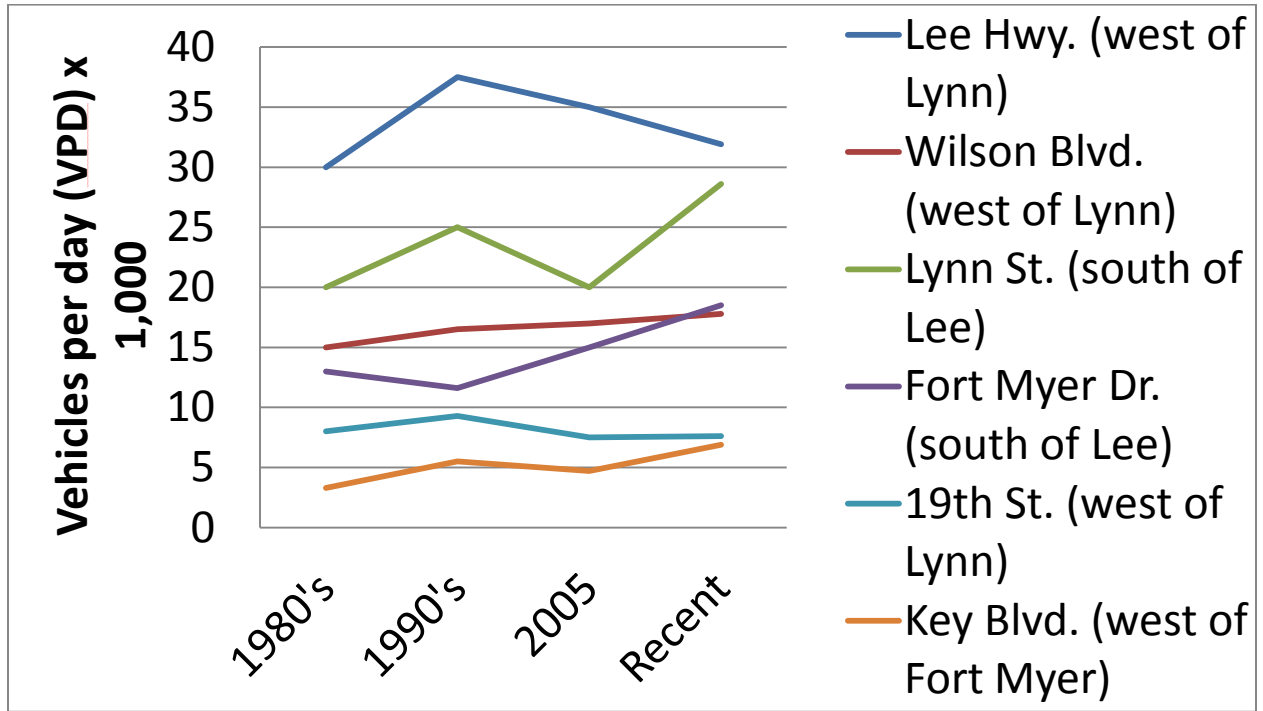
Mapping of traffic routes and volumes passing through Rosslyn, morning peak.



Principal traffic routes on Rosslyn streets connecting regional highways and bridges

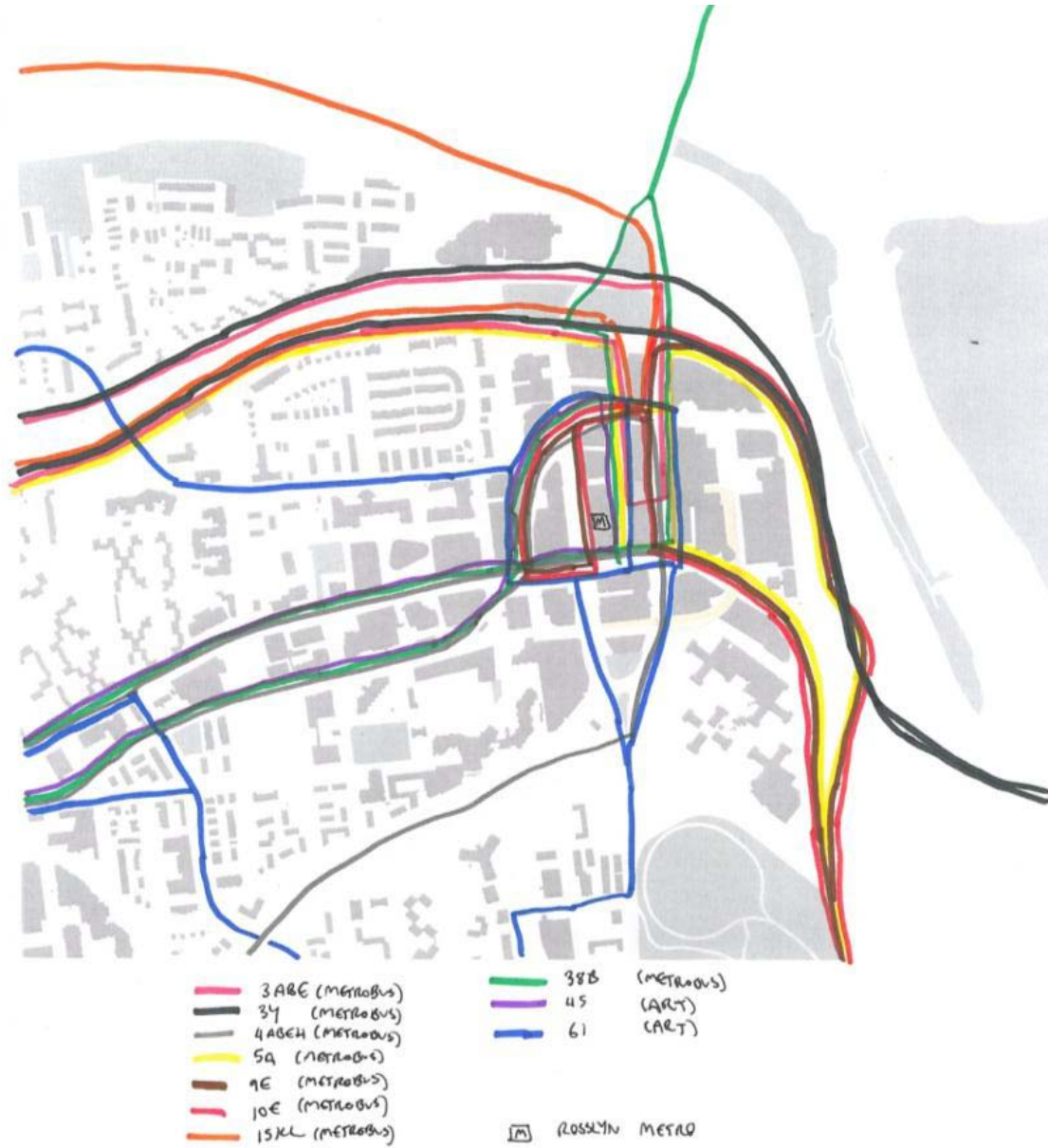


Traffic volume trends in Rosslyn



- Traffic volume an influential factor in pedestrian and bike levels of service
- Daily traffic counts over 20,000 tend to challenge walkability
- Overall levels have risen little over last 25 years, but there have been notable increases on Lynn, Ft. Myer, Key Blvd.

Principal bus routes in Rosslyn



F. Public Parks and Open Space Existing Conditions

Rosslyn is at once endowed with a significant amount of public open spaces within and around it, and lacking adequate connections, design and programming to make full use of these vital assets. Many public open spaces and landscaped areas along streets were originally designed consistent with the suburban auto-oriented office environment Rosslyn once was, than the vibrant and pedestrian-oriented urban place it is today. Park and plaza spaces thus often do not support the range of activities that Rosslyn's increasingly diverse population desires. Rosslyn also is adjacent to the Custis Trail, Mount Vernon Trail, Potomac Heritage Trail, and other great walking and biking corridors, yet its connections to these can be missing, hard to navigate, unpleasant or unsafe. More specific landscape conditions related to Rosslyn's significant public spaces are as follows:

Existing features:

- *Gateway Park* – Rosslyn's largest and most prominent park is also one of its hardest to use effectively. Which it significantly diminishes the presence of I-66, its location above the highway, at Rosslyn's edge, separated from Rosslyn's core by streets busy with traffic, and visually separated by skywalk structures, makes it inconvenient and uninviting for many people to use. Its extensively designed network of terraces, walks and plantings is well-intentioned but limits flexibility of use. The park's division by North Fort Myer Drive, and concrete structures that bridge this divide, further limit its usability and accessibility. Apart from a successful annual jazz festival and other programmed special events (including Friday film festivals), the park receives relatively little daily use except as a periodic food distribution point for homeless people – a role that is generally appreciated and tolerated by Rosslyn stakeholders. In addition to its potential to accommodate destination activities, Gateway Park is important as a highly visible gateway to Rosslyn and the District of Columbia, and a connector to the George Washington Memorial Parkway, Key Bridge, and open spaces that provide fine views of the Potomac and are designated as potential national memorial sites. A previously initiated planning and implementation effort to rethink and develop a specific master plan for Gateway Park will resume after completion of the Rosslyn Sector Plan Update. The sector planning effort should provide guidance to the park planning effort so that Gateway Park may most effectively become a more active and important place for people in Rosslyn.
- *Freedom Park* – The transformation of Rosslyn's former loop road into a public park invariably draws comparisons to New York City's more recent High Line. The park's high elevation over Wilson Boulevard and North Lynn Street offer dramatic views toward Washington's National Cathedral and Monumental Core as well as Rosslyn's own people, streets and buildings. Freedom Park, however, needs more appeal as a destination and greater access to and from adjacent streets and buildings to function as successfully as the High Line's public spaces. While Monday Properties does maintain the attractively landscaped park and provides elevator access via its 1100 Wilson Boulevard Building, and the nearby Artisphere also provides access, most people on the street lack visual connection and motivation to ascend to the park. Displays formerly occupied by the Newseum are gone. The locations where Freedom Park's walks do connect to streets at grade – midblock along North Kent Street, at the edge of River Place along North Lynn Street, and at 17th Street between North Fort Myer Drive and North Lynn Street – are also relatively hidden. This plan's analysis process considered whether Rosslyn was better off with Freedom

Park's elevated structure removed and transformed into a grade-level park space and/or street. While removing the structure could improve the pedestrian environment where North Kent Street meets Wilson Boulevard, on balance the preservation and enhancement of the park offer more compelling opportunities. Several factors offered persuasive reason to keep the elevated park:

- potential to extend the park around a larger themed "Green Circle" of much greater prominence;
 - its excellent views;
 - its significant amount of open space, with potential augmentation by underutilized space on the bridge over North Fort Myer Drive;
 - its very quirkiness as a unique Rosslyn artifact;
 - its potential to integrate access and activity with future redevelopment along its extended path; presence of occupied building space below; and
 - the significant cost of demolishing its concrete bridge structure.
- *Highland Park* – This park toward the west end of the RCRD, adjacent to the former Wilson Elementary School and a county fire station, is the subject of significant community interest in enhancing the park by using portions of the school property. A separate community planning process will explore opportunities for this site.
 - *Streets* – As discussed in the Urban Design section above, Rosslyn's streets vary in their extent of landscape and appeal as public spaces. While many streets are lined with street trees and ornamental plantings, others are devoid of trees. Where retail and other active edges are lacking, direct programming of sidewalk spaces with recreation, "parklets," interpretive signage, public art, fountains or other outdoor activities could transform sidewalks into interesting places of activity and community. Current and potential programming of such spaces offers opportunity for a variety of stakeholders to participate in activating Rosslyn's public spaces – particularly the Rosslyn Business Improvement District (BID), and individual property and business owners and artists, not to mention Arlington's Department of Parks and Recreation.

Approved features:

- *Central Place Plaza* – This designed, approved and funded public plaza will be created between the two approved Central Place buildings, across North Moore Street from the Metro station. Heavy foot traffic flowing to and from the station, and new ground floor retail and dining offerings fronting this space will make it an instant center of activity and identity. While large enough to support a range of programming, the space should be complemented by others in and around Rosslyn capable of accommodating larger outdoor activities such as sports, concerts and festivals.
- *Corridor of Light* – This designed, approved, and partially funded public art installation will line North Lynn Street from Arlington Boulevard to the Key Bridge. Pylons or sculptures known as Luminous Bodies of two different sizes will emit colored light in changing patterns. The installation will distinguish North Lynn as a local and regional street of special prominence, and enhance the street as a public destination and place to walk. Any opportunities to widen North Lynn's sidewalks would enhance opportunity for people to appreciate the installation and for placement of outdoor dining and other sidewalk furniture amidst the Luminous Bodies.
- *Public Art* – Rosslyn is already distinguished by an extensive collection of public art, including Dark Star Park and Cupid's Garden. The Artisphere's presence in Rosslyn further expands its art

offerings to include performing arts and gallery displays. There remains opportunity, however, to add more public art and/or include public art that is temporary in nature. A recent, well appreciated example in Rosslyn was the “yarn bombing” of certain street trees and installations of colored flags on skywalks. Arlington’s arts supporters would welcome greater opportunity to see more art produced and displayed in Rosslyn.

Currently/Previously Proposed Concepts

- *Rosslyn Plaza* – The proposed redevelopment of Rosslyn Plaza, currently under site plan review, has been prioritized as an important opportunity for a major new public park/plaza space as well as a central link in the Esplanade (described further below), public streets and walks integrated with Rosslyn’s street network, a connecting point for a multi-use path bridge to the GWMP, and eventually a possible connection to a landscaped deck area over I-66 (also described further below). Site dimensions offer opportunity for a park or plaza space larger than Central Place Plaza and large enough to accommodate activities such as ice skating and farmer’s markets. Such destination uses would be appropriate for the public space, given their potential to draw activity toward its location off major existing walking routes. Significant opportunities for views toward Roosevelt Island, Georgetown and the Lincoln Memorial are available.
- *The Esplanade* – This concept, which precedes the current planning effort, would create a pedestrian promenade along Rosslyn’s edge with I-66 and VA-110, taking advantage of impressive Potomac River corridor views and relatively consistent right of way opportunity. The Esplanade could make valuable connections for recreation and everyday access on foot and bike, as far northwest as Gateway Park and as far south as River Place and the Marine Corps Memorial and adjacent GWMP open space. Creation of the Esplanade would likely need to occur in incremental segments related to adjacent redevelopment and infrastructure improvements. Individual segments, however, will offer significant value as destinations in their own right and potential pathway links to a bridge over I-66, the Green Circle and Rosslyn’s street network.
- *George Washington Memorial Parkway* – The Parkway offers a variety of special destinations and experiences surprisingly close to Rosslyn, from the well-known Theodore Roosevelt Memorial and Mount Vernon Trail to little-known viewpoints overlooking the Key Bridge. A long-term County- and privately-led effort to create a public boathouse with river access and boat storage just off Gateway Park is making significant progress. The National Park Service (NPS) and Arlington County have established a productive working relationship exploring this and other possibilities to provide park facilities in partnership. NPS, while supportive of interest in making the GWMP more accessible from Rosslyn for everyday use, also is committed to maintaining certain view corridors and other park characteristics supporting its mission as a resource to the nation. Portions of the parkway southeast of Rosslyn contain significant areas of parkland that are currently inaccessible to pedestrians due to a web of regional highway ramps and the Blue Line Metro right of way. Adding multi-use path bridges over these obstacles could offer major new park program opportunities (such as sports fields) to Rosslyn, and make strategic path connections to the Mount Vernon Trail and Theodore Roosevelt Bridge.
- *Landscaped deck and/or bridges over I-66 and GWMP* – Concepts for crossing and/or covering the highway’s busy, noisy intrusion between Rosslyn and the Potomac have been discussed for over fifteen years. A landscaped deck would be highly desirable for adding park land, diminishing the visual and noise impacts of I-66, and enhancing views from Rosslyn public and

private spaces. It would also pose tremendous costs that currently do not appear to have realistic private or public funding sources. This planning process intends to maintain opportunity for feasible connections to a future landscaped deck, while acknowledging that it is not likely to be a near-term possibility or funding priority given other plan priorities in Rosslyn. A multi-use path bridge over I-66, however, could more realistically be funded and designed, and would offer compelling recreational connections. Analysis has identified several potential bridge configurations, typically connecting to North Arlington Ridge Road and the proposed Esplanade near 19th.

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